Kane County Division of Transportation

Longmeadow Parkway Section C1 – Contract 63955

Letting Date: October 2nd, 2018 @ 9:00 am

Addendum #1 – September 25, 2018

Addendum #1, as issued by the Kane County Division of Transportation on September 25, 2018, includes the following revisions and clarifications:

Special Provisions

- BLR 12200a form (page 3 of 6) – Revised pay item 52200020, TEMP SOIL RETEN SYSTM from 3,040 Sq. Ft. to 4,580 Sq. Ft.
- Page 20 – Revised pre-approved manufacturer of Form Liner Textured Surface to Custom Rock Formliner, Pattern Number 1208, Pattern Name: Drystack.

Plans

- Sheet 006 - Revised pay item 52200020, TEMP SOIL RETEN SYSTM from 3,040 Sq. Ft. to 4,580 Sq. Ft.
- Sheets 018 & 019 – Included note that requires erosion control measures to be installed prior to beginning tree removal operations.
- Sheet 035 – Included Temporary Soil Retention System between MSE wall 2 and the Temporary Pedestrian Enclosure.
- Sheet 036 – Included Temporary Soil Retention System between MSE wall 2 and the Temporary Pedestrian Enclosure.
- Sheet 126 – Revised structural steel designation for balcony steel on Fox River bridge from a W21x107 to a W21x101.
- Sheet 141 – Revised pre-approved manufacturer of Form Liner Textured Surface to Custom Rock Formliner, Pattern Number 1208, Pattern Name: Drystack.
- Sheet 158 – Revised pre-approved manufacturer of Form Liner Textured Surface to Custom Rock Formliner, Pattern Number 1208, Pattern Name: Drystack.
- Sheet 185 – Included design stress and steel type for Pedestrian Truss Superstructure; fy = 50,000 psi (M270 Grade 50W) - Truss
- Sheet 186
    - Revised typical cross section to show stringers under deck
    - Added notes under “Truss Manufacturer”:
        - Bridge Decking shall be nominal 3” thick select structural timber in accordance with Section 1007.03 of the IDOT Standard Specifications. F_{bo} = 1.40 ksi minimum. Maximum stringer spacing 1’-8”.
        - The truss manufacturer shall design connection of timber deck to stringers and submit sealed calculations with shop drawings.

Bidder Questions

1. RE: Pedestrian Truss Superstructure – Fence Type

   Question: There is also nothing of note regarding the fencing, other than a note that says chain link fence (typ.).

   Answer: Chain link fence fabric is covered under Section 1006.27 of Std. Specifications. The Bicycle railing sheet calls for 9-gauge wire, 2" mesh chain link fabric. This is consistent with the 0.148" diameter called out in the specs.

2. RE: Bridge Deck Thin Polmer Overlay 3/8"

   Question: The bridge deck and approach pavements are to receive a 3/8" thin polymer overlay. Please address the relative position (elevation) of the overlay with regards to the expansion joints and scuppers that will be embedded in the bridge deck and approach pavement concrete. Is the intent for the overlay to be 3/8" above the surface of the joints and scuppers or is the intent to be flush (at the same elevation)?

   Answer: Regarding the thickness of overlay, the intent is to have the top of overlay 3/8" above the 8" concrete deck (and surface of joints and scuppers). For polymer overlay in Illinois, it is treated as a sealer application.

3. RE: Pedestrian Truss Superstructure – Painting of Weathering Steel

   Question: What is the desired finish of the bridge, weathering steel or painted? If weathering steel, will the 10' ends require painting? We have assumed weathering steel and no painted ends but want to confirm.

   Answer: Weathering Steel. It was **not** our intent to require the painting at the ends. Bridge Manual Section 3.1.3, general note 19 addresses painting near deck joints on new weathering steel. Our interpretation of this was that it applies to beam bridges. We do not have typical deck joints here.

4. RE: Pedestrian Truss Superstructure – Toe Rail

   Question: Will the bridge require a toe rail? A rub rail is shown in the section-thru, however no toe rail is shown. Please advise if a toe rail is required.

   Answer: Toe plate is required per GBSP33: *Railing. The railing shall consist of a smooth rub rail, a toe plate and misc. elements, all located on the inside face of the truss.*

RETURN WITH BID

Bidder's Proposal for making Entire Improvements

Item No.	Items	Unit	Quantity	Unit Price	Total
52100510	ANCHOR BOLTS 3/4	EACH	22		
52100520	ANCHOR BOLTS 1	EACH	36		
52100530	ANCHOR BOLTS 1 1/4	EACH	108		
52200020	TEMP SOIL RETEN SYSTM	SQ FT	4,580		
52200500	MECH ST EARTH RET WL	SQ FT	30,679		
54213657	PRC FLAR END SEC 12	EACH	8		
54213660	PRC FLAR END SEC 15	EACH	2		
54213663	PRC FLAR END SEC 18	EACH	3		
54213666	PRC FLAR END SEC 21	EACH	2		
54213669	PRC FLAR END SEC 24	EACH	2		
54213681	PRC FLAR END SEC 36	EACH	3		
5421D021	P CUL CL D 1 21 TEMP	FOOT	155		
5422D030	P CUL CL D 2 30 TEMP	FOOT	25		
542A0220	P CUL CL A 1 15	FOOT	114		
542A0223	P CUL CL A 1 18	FOOT	119		
542A0226	P CUL CL A 1 21	FOOT	174		
542A1057	P CUL CL A 2 12	FOOT	32		
542A1069	P CUL CL A 2 24	FOOT	42		
542A1081	P CUL CL A 2 36	FOOT	44		
550A0050	STORM SEW CL A 1 12	FOOT	137		
550A0090	STORM SEW CL A 1 18	FOOT	273		
550A0340	STORM SEW CL A 2 12	FOOT	1,591		
550A0360	STORM SEW CL A 2 15	FOOT	255		
550A0380	STORM SEW CL A 2 18	FOOT	291		
550A0450	STORM SEW CL A 2 36	FOOT	178		
58700300	CONCRETE SEALER	SQ FT	3,379		
59100100	GEOCOMPOSITE WALL DR	SQ YD	267		
60108204	PIPE UNDERDR T 2 4	FOOT	2,422		
60200805	CB TA 4 DIA T8G	EACH	1		
60201340	CB TA 4 DIA T24F&G	EACH	10		
60218400	MAN TA 4 DIA T1F CL	EACH	16		
60221100	MAN TA 5 DIA T1F CL	EACH	1		
60236800	INLETS TA T11F&G	EACH	6		
60237470	INLETS TA T24F&G	EACH	4		
60605000	COMB CC&G TB6.24	FOOT	3,580		
60619600	CONC MED TSB6.12	SQ FT	12,488		
63000001	SPBGR TY A 6FT POSTS	FOOT	12.5		
63100045	TRAF BAR TERM T2	EACH	1		
63100085	TRAF BAR TERM T6	EACH	2		
63100167	TR BAR TRM T1 SPL TAN	EACH	1		
67000400	ENGR FIELD OFFICE A	CAL MO	21		
67100100	MOBILIZATION	L SUM	1		
67201000	SEAL ABAN WATER WELLS	EACH	1		
70300904	PAVT MARK TAPE T4 4	FOOT	218		
70300924	PAVT MARK TAPE T4 24	FOOT	44		
70400100	TEMP CONC BARRIER	FOOT	350		

1

Manufacturer	Pattern Number	Pattern Name
Custom Rock Formliner 2020 West 7th Street St. Paul, Minnesota 55116 (651) 699-1345 info@customrock.com	Pattern Number 1208	Drystack Stone

Pre-approval of the form liner does not include material acceptance at the job site.

The form ties shall be made of either metal or fiberglass. Metal ties, which result in a portion of the tie permanently embedded in the concrete, shall be designed to separate at least one inch back from finished surface, leaving only a neat hole that can be plugged with patching material. Contractor shall submit the type of form ties to the Engineer for approval prior to use in this work.

Concrete used for the cast-in-place concrete designated to receive form liner textured surfaces shall contain a high range water-reducing admixture according to Article 1021.03(c) of the "Standard Specifications" to obtain a 5" to 7" slump.

Submittals: Upon approval of the form liner plans and installation procedure in accordance with Article 503.06(a), the Contractor shall submit three 6' by 6' (minimum) mock-up cast concrete panels of the simulated stone masonry finish of the Form Liner Textured Surface for approval by the Engineer. Include an area to demonstrate wall mold butt joint. The mock-up panels shall also include the concrete staining and anti-graffiti coating as indicated in the Special Provision for STAINING CONRETE STRUCTURES and ANTI- GRAFFITI PROTECTION SYSTEM.

The sample panels shall be delivered and positioned on the job site at a location to be determined by the Engineer. The approved form liners shall be used throughout the project to replicate natural stone surfaces unless otherwise noted in the plans. The approved mock-ups shall be the standard for replicated natural stone surfaces where required throughout the project.

Construction Requirements: The work shall be performed according to the applicable portions of Article 503.06 of the "Standard Specifications" with emphasis on Article 503.06(a), except as modified herein, and the following:

The form liners shall be installed according to the manufacturers' recommendations to achieve the highest quality concrete appearance possible. The form liners shall withstand the concrete placement pressures without leakage, physical or visual defects.
The Contractor shall clean the form liners, removing any buildup prior to each use. The Contractor shall inspect each form for blemishes or tears and make repairs as needed following manufacturer's recommendations.

The Contractor shall install the form liners with less than ¼ inch separation between them. The molds shall be attached securely to the forms following manufacturer's recommendations. The

SPECIALTY ITEM	SPECIAL PROVISION	CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY
		44000100	PAVEMENT REMOVAL	SQ YD	800
		48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	125
		48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SQ YD	80
		50105220	PIPE CULVERT REMOVAL	FOOT	283
		50200100	STRUCTURE EXCAVATION	CU YD	3,285
		50200300	COFFERDAM EXCAVATION	CU YD	2,521
		50201121	COFFERDAM (TYPE 2) (LOCATION - 1)	EACH	1
		50201122	COFFERDAM (TYPE 2) (LOCATION - 2)	EACH	1
		50201123	COFFERDAM (TYPE 2) (LOCATION - 3)	EACH	1
		50300225	CONCRETE STRUCTURES	CU YD	1,720.4
		50300255	CONCRETE SUPERSTRUCTURE	CU YD	2,349.3
		50300265	SEAL COAT CONCRETE	CU YD	916
	•	50300285	FORM LINER TEXTURED SURFACE	SQ FT	8,314
		50300300	PROTECTIVE COAT	SQ YD	4,072
		50301350	CONCRETE SUPERSTRUCTURE (APPROACH SLAB)	CU YD	285.3
		50500105	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1
		50500505	STUD SHEAR CONNECTORS	EACH	17,208
		50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	949,521
		50800515	BAR SPLICERS	EACH	210
		50900805	PEDESTRIAN RAILING	FOOT	1,211
		50901720	BICYCLE RAILING	FOOT	40
		50901750	PARAPET RAILING	FOOT	1,200
		51200957	FURNISHING METAL SHELL PILES 12" X 0.250"	FOOT	741
		51201600	FURNISHING STEEL PILES HP12X53	FOOT	3,762
		51201610	FURNISHING STEEL PILES HP12X63	FOOT	9,502
		51202305	DRIVING PILES	FOOT	14,005
		51203200	TEST PILE METAL SHELLS	EACH	2
		51203600	TEST PILE STEEL HP12X53	EACH	2
		51203610	TEST PILE STEEL HP12X63	EACH	3
		51204650	PILE SHOES	EACH	196
		51500100	NAME PLATES	EACH	3
		52000110	PREFORMED JOINT STRIP SEAL	FOOT	69
		52100020	ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	18
		52100510	ANCHOR BOLTS, 3/4"	EACH	22
		52100520	ANCHOR BOLTS, 1"	EACH	36
		52100530	ANCHOR BOLTS, 1 1/4"	EACH	108
		52200020	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	1 4,580
		52200500	MECHANICALLY STABILIZED EARTH RETAINING WALL	SQ FT	30,679

USER NAME = Mike Moes	DESIGNED - JMS/PFR	REVISED - 1 9/25/18	
	DRAWN - PFR	REVISED -	
PLOT SCALE = 2.0000 ' / in.	CHECKED - KDF	REVISED -	
PLOT DATE = 9/24/2018	DATE - 8/31/2018	REVISED -	

KANE COUNTY
DIVISION OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NTS | SHEET 2 OF 6 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2298	13-00215-20-BR	KANE	239	6
		CONTRACT NO. 63955		
		ILLINOIS FED. AID PROJECT		

LEGEND

- PAVEMENT REMOVAL
- AGGREGATE SHOULDER REMOVAL (INCLUDED IN EARTH EXCAVATION
- TREE REMOVAL, ACRES
- FENCE REMOVAL / LINEAR ITEM REMOVAL
- XXXX CULVERT/TREE REMOVAL
- ADJ R ADJUST / REMOVE STRUCTURE

B R U N N E R F O R E S T
P R E S E R V E

℄ PROP. LONGMEADOW PARKWAY

EX. ROW

2185+00

2190+00

2200+00

2195+00

T.E.

L200+00

L110+00

MATCH LINE STA. 2194+50

MATCH LINE STA. 2207+50

SECTION B2 (BY OTHERS)
IDOT CONTRACT 61E05

BEGIN IMPROVEMENT
STA. 2188+00.00

TREE REMOVAL, 0.898 ACRES

N

REMOVAL PLAN GENERAL NOTES

1. TREE MEASUREMENTS FOR TREE REMOVAL, ACRES WERE TAKEN IN FALL OF 2013. CONTRACTOR SHALL ACCOUNT FOR TREE GROWTH IN THEIR BID PRICE.
2. AGGREGATE SHOULDER REMOVAL IS TO BE INCLUDED AS EARTH EXCAVATION.
3. THE CONTRACTOR SHALL INSTALL EROSION CONTROL MEASURES IN ACCORDANCE WITH THE EROSION CONTROL PLANS PRIOR TO BEGINNING TREE REMOVAL. MEASURES SHALL BE TAKEN TO PREVENT SEDIMENT AND DERBIS FROM ENTERING THE FOX RIVER. ADDITIONAL EROSION CONTROL METHODS SHALL BE IMPLEMENTED AS DIRECTED BY THE ENGINEER. (1)

ADDENDUM #1

LOCAL COORDINATE SYSTEM

FILE NAME = L:\KANECO\13296-02_LongmeadowPkwy\Draw\CADD_Sheets\SectionC_sht_rem-Long01_cmt.dgn

CMT
License No. 184-000613

USER NAME = Mike Moes	DESIGNED - JMS	REVISED - 1 9/25/18
	DRAWN - JPZ	REVISED -
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PLOT DATE = 9/25/2018	DATE - 8/31/2018	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

REMOVAL PLAN
LONGMEADOW PARKWAY

SCALE: 1"=50' | SHEET 1 OF 2 SHEETS | STA. 2188+00 TO STA. 2207+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2298	13-00215-20-BR	KANE	239	18
		CONTRACT NO. 63955		
	ILLINOIS FED. AID PROJECT			

LEGEND

- PAVEMENT REMOVAL
- AGGREGATE SHOULDER REMOVAL (INCLUDED IN EARTH EXCAVATION
- TREE REMOVAL, ACRES
- FENCE REMOVAL / LINEAR ITEM REMOVAL
- ×××× CULVERT/TREE REMOVAL
- ADJ R ADJUST / REMOVE STRUCTURE

N

℄ PROP. LONGMEADOW PARKWAY

FOX RIVER

TREE REMOVAL, 0.908 ACRES

BIKE PATH REMOVAL
350 SQ YD

TREE REMOVAL, 0.919 ACRES

TREE REMOVAL, 0.204 ACRES

CULVERT REMOVAL
(12" CMP)

AGGREGATE SHOULDER REMOVAL
(INCLUDED IN EARTH EXCAVATION)
REQUIRED FOR FOX RIVER PATH RE-ROUTE

TREE REMOVAL, 0.609 ACRES

ABANDON EX.
WELL (SEE NOTE 2)

MS:
3x11"

ADJUST EX. ABANDONED
WELL (SEE NOTE 3)

MS:
3x24"

EXISTING RETAINING WALL
TO REMAIN.

SEE NOTE 5

MS:
2x24"

MS:
9",10"

MS: 2x16"

MATCH LINE STA. 2207+50

2210+00

2215+00

2220+00

SANDBLOOM ROAD

BOLZ ROAD

ANGELINA PLACE

PAVEMENT REMOVAL
800 SQ YD

REMOVE DOCK

FENCE REMOVAL
(418 FOOT) INCLUDED
IN THE COST OF CLEARING

CULVERT REMOVAL
(12" CMP)

CULVERT REMOVAL
(36" X 24 ELLIPTICAL CMP)

ADJUST EX. ABANDONED
WELL (SEE NOTE 4)

OLD WILLIAMS ROAD

ADDENDUM #1

NOTES

1. SEE SHEET 18 FOR REMOVAL PLAN GENERAL NOTES.
2. WELL DIAMETER = 5" (ASSUMED), DEPTH = 80' (ASSUMED) REFER TO SP FOR "SEALING ABANDONED WATER WELLS."
3. 5" WELL FILLED WITH BENTONITE (DU089-0257-06). REFER TO SP FOR "ADJUSTMENTS, SPECIAL".
4. 5" WELL (ASSUMED) FILLED WITH BENTONITE (DU089-0425-01). REFER TO SP FOR "ADJUSTMENTS, SPECIAL".
5. TREES SHOWN FOR REMOVAL THAT EXIST WITHIN 25 FEET OF THE EAST BANK OF THE FOX RIVER SHALL HAVE THEIR STUMPS REMAIN IN PLACE FOLLWING REMOVAL OF THE TREE.
6. EXISTING SEPTIC SYSTEMS MAY BE ENCOUNTERED DURING CONSTRUCTION OF THE PROPOSED DETENTION BASIN. SEE SPECIAL PROVISION FOR REMOVING EXISTING SEPTIC TANK.
7. (1) THE CONTRACTOR SHALL INSTALL EROSION CONTROL MEASURES IN ACCORDANCE WITH THE EROSION CONTROL PLANS PRIOR TO BEGINNING TREE REMOVAL. MEASURES SHALL BE TAKEN TO PREVENT SEDIMENT AND DERBIS FROM ENTERING THE FOX RIVER. ADDITIONAL EROSION CONTROL METHODS SHALL BE IMPLEMENTED AS DIRECTED BY THE ENGINEER.

LOCAL COORDINATE SYSTEM

FILE NAME = L:\KANECO\13296-02_Longmeadow\Plwg\Draw\CADD_Sheets\Section\C-sht-rem-Long-02_cmt.dgn

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License No.: 184-000613

USER NAME = Mike Moes	DESIGNED - JMS	REVISED - (1) 9/25/18
	DRAWN - JPZ	REVISED -
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PLOT DATE = 9/25/2018	DATE - 8/31/2018	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

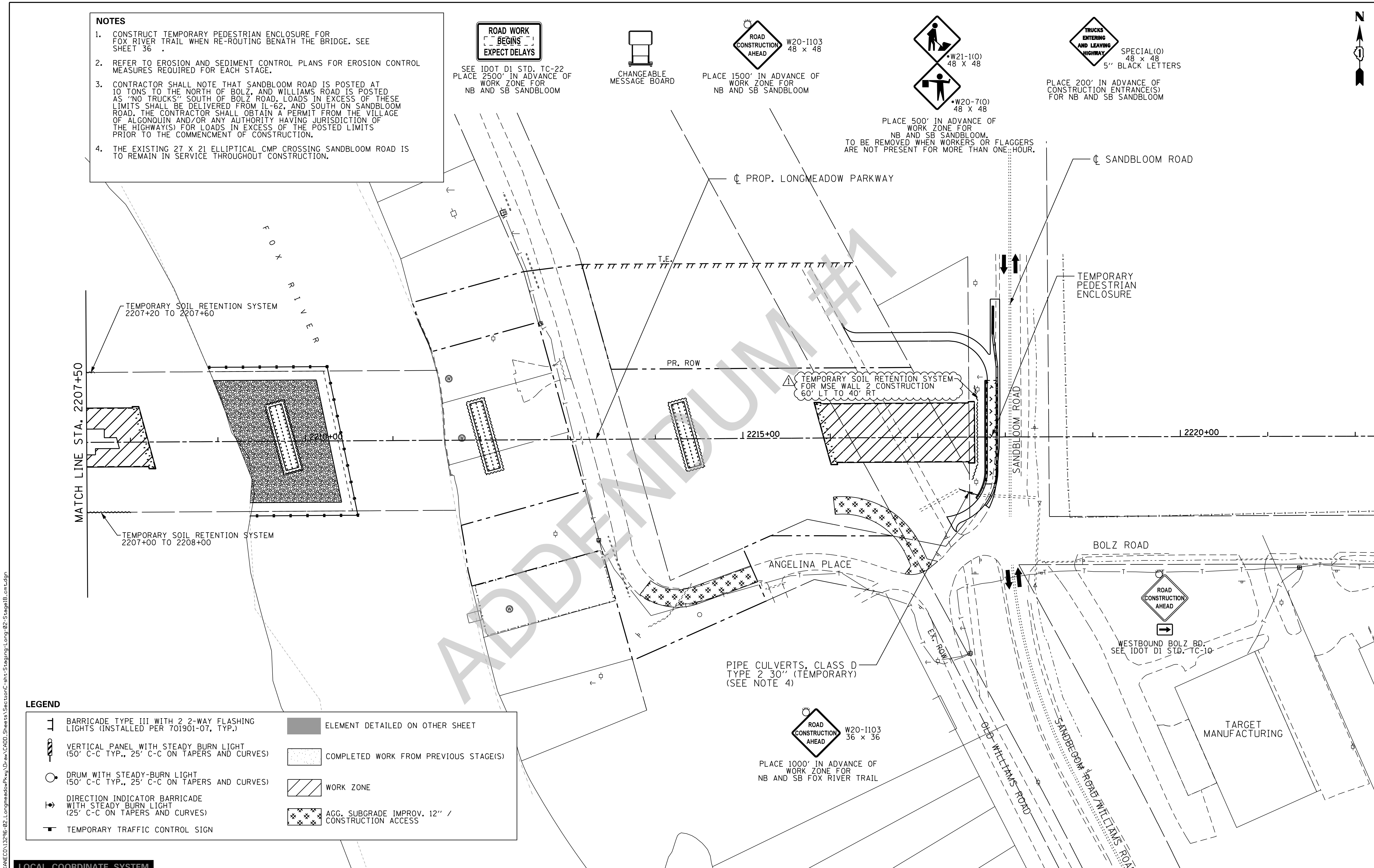
REMOVAL PLAN
LONGMEADOW PARKWAY

SCALE: 1"=50' | SHEET 2 OF 2 SHEETS | STA. 2207+50 TO STA. 2221+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2298	13-00215-20-BR	KANE	239	19
		CONTRACT NO. 63955		
		ILLINOIS FED. AID PROJECT		

NOTES

1. CONSTRUCT TEMPORARY PEDESTRIAN ENCLOSURE FOR FOX RIVER TRAIL WHEN RE-ROUTING BENATH THE BRIDGE. SEE SHEET 36 .
2. REFER TO EROSION AND SEDIMENT CONTROL PLANS FOR EROSION CONTROL MEASURES REQUIRED FOR EACH STAGE.
3. CONTRACTOR SHALL NOTE THAT SANDBLOOM ROAD IS POSTED AT 10 TONS TO THE NORTH OF BOLZ, AND WILLIAMS ROAD IS POSTED AS "NO TRUCKS" SOUTH OF BOLZ ROAD. LOADS IN EXCESS OF THESE LIMITS SHALL BE DELIVERED FROM IL-62, AND SOUTH ON SANDBLOOM ROAD. THE CONTRACTOR SHALL OBTAIN A PERMIT FROM THE VILLAGE OF ALGONQUIN AND/OR ANY AUTHORITY HAVING JURISDICTION OF THE HIGHWAY(S) FOR LOADS IN EXCESS OF THE POSTED LIMITS PRIOR TO THE COMMENCMENT OF CONSTRUCTION.
4. THE EXISTING 27 X 21 ELLIPTICAL CMP CROSSING SANDBLOOM ROAD IS TO REMAIN IN SERVICE THROUGHOUT CONSTRUCTION.



LEGEND

BARRICADE TYPE III WITH 2 2-WAY FLASHING LIGHTS (INSTALLED PER 701901-07, TYP.)	ELEMENT DETAILED ON OTHER SHEET
VERTICAL PANEL WITH STEADY BURN LIGHT (50' C-C TYP., 25' C-C ON TAPERS AND CURVES)	COMPLETED WORK FROM PREVIOUS STAGE(S)
DRUM WITH STEADY-BURN LIGHT (50' C-C TYP., 25' C-C ON TAPERS AND CURVES)	WORK ZONE
DIRECTION INDICATOR BARRICADE WITH STEADY BURN LIGHT (25' C-C ON TAPERS AND CURVES)	AGG. SUBGRADE IMPROV. 12" / CONSTRUCTION ACCESS
TEMPORARY TRAFFIC CONTROL SIGN	

LOCAL COORDINATE SYSTEM

FILE NAME = L:\KANECO\13296-02_LongmeadowPkwy\Draw\CADD_Sheets\Section\C-sht-Staging-Long-02-Stage1B_cmt.dgn

CMT	USER NAME = Mike Moes	DESIGNED - JMS/MPM	REVISED - 1 9/25/18	KANE COUNTY DIVISION OF TRANSPORTATION	SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL LONGMEADOW PARKWAY STAGE 1B
		DRAWN - MPM	REVISED -		
	PLOT SCALE = 100.0000 '/ in.	CHECKED - KDF	REVISED -		
License No. 184-000613 © Copyright CMT, Inc.	PLOT DATE = 9/24/2018	DATE - 8/31/2018	REVISED -		SCALE: 1"=50' SHEET 2 OF 3 SHEETS STA. 2207+50 TO STA. 2221+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2298	13-00215-20-BR	KANE	239	35
		CONTRACT NO. 63955		
	ILLINOIS FED. AID PROJECT			

NOTES

1. REFER TO SPECIAL PROVISIONS FOR TEMPORARY PEDESTRIAN ENCLOSURE REQUIREMENTS.
2. ENCLOSURE SHALL EXTEND A MINIMUM OF 25' BEYOND THE THEORETICAL EDGE OF THE BRIDGE DECK ON BOTH THE NORTH AND SOUTH SIDE OF LONGMEADOW PARKWAY.
3. IDOT STANDARD 701501-06 SHALL BE UTILIZED FOR LANE CLOSURES REQUIRED FOR THE CONSTRUCTION OF THE TEMPORARY PEDESTRIAN ENCLOSURE/FOX RIVER TRAIL DETOUR AND ANY OTHER WORK ON THE FOX RIVER BRIDGE AND MSE WALLS.
4. BIKE PATH PAVEMENT AND TEMPORARY CONCRETE BARRIER PAID FOR SEPARATLY.
5. SIGNAGE AND BARRICADES REQUIRED FOR THE FOX RIVER TRAIL DETOUR SHALL BE PAID FOR AS "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

N

TEMPORARY EASEMENT

DETOUR
M4-9cL
30 X 24

TRAIL CLOSED
R11-2 (MOD.)
48 X 30

DETOUR
M4-10L
48 X 18

PR. ROW

FOX RIVER TRAIL

END DETOUR
M4-8a
24 X 18

IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3

HMA SURFACE COURSE, MIX D, N50 3"
AGGREGATE BASE COURSE, TYPE B 6"

53'
10'

W1-1R(O)
18 X 18

TEMPORARY SOIL RETENTION SYSTEM FOR MSE WALL 2 CONSTRUCTION
60' LT TO 40' RT

TEMPORARY CONCRETE BARRIER

W1-1L(O)
18 X 18

℄ SANDBLOOM ROAD

℄ PROP. LONGMEADOW PARKWAY

SANDBLOOM ROAD

TEMPORARY CONCRETE BARRIER, PINNED

51'

2215+00 | 2216 | 2217 | 2218 | 2219 | 2220+00

TRAIL CLOSED
R11-2 (MOD.)
48 X 30

DETOUR
M4-10R
48 X 18

50+00

10'

END DETOUR
M4-8a
24 X 18

DETOUR
M4-9cR
30 X 24

ANGELINA PLACE

BOLZ ROAD

ADDENDUM #1

LOCAL COORDINATE SYSTEM

FILE NAME = L:\KANECO\13295-02_Longmeadow\Plwy\Draw\CADD_Sheets\SectionC-ant-Staging-Long-05-StageIB.cmt.dgn

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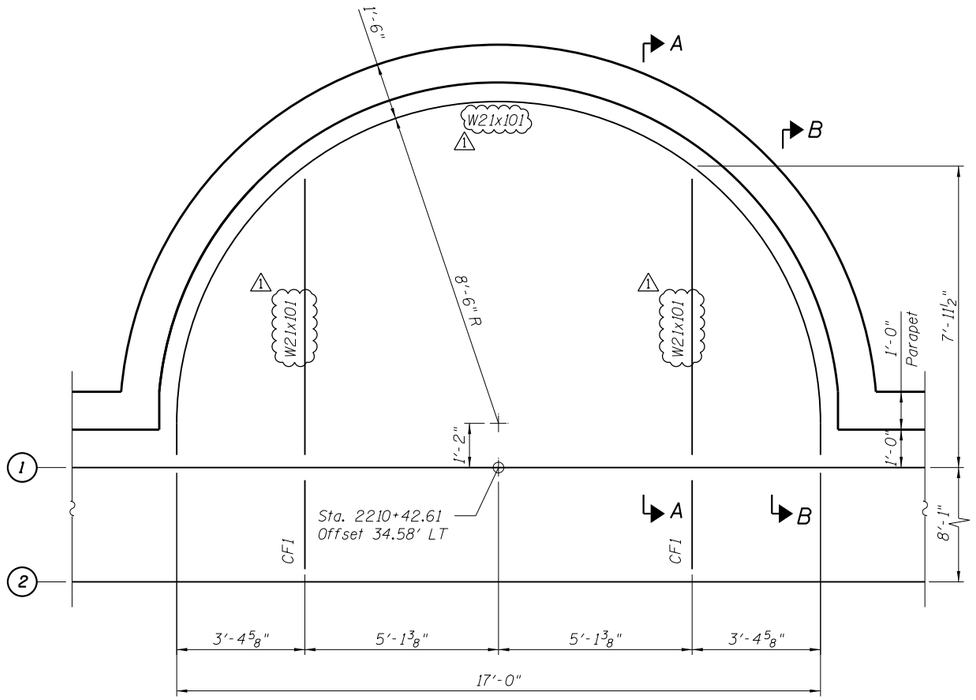
	DRAWN - MPM	REVISED -
PLOT SCALE = 40.0000 ' / in.	CHECKED - KDF	REVISED -
PLOT DATE = 9/24/2018	DATE - 8/31/2018	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

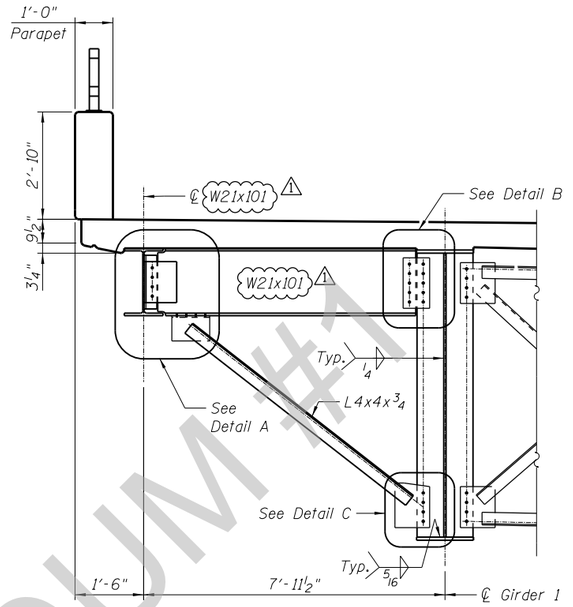
SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL
LONGMEADOW PARKWAY
STAGE 1B – FOX RIVER TRAIL

SCALE: 1"=20' | SHEET 3 OF 3 SHEETS | STA. - | TO STA. -

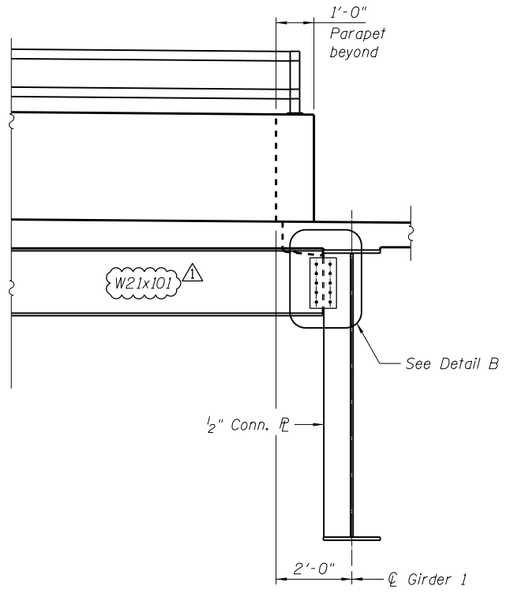
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2298	13-00215-20-BR	KANE	239	36
		CONTRACT NO. 63955		
		ILLINOIS FED. AID PROJECT		



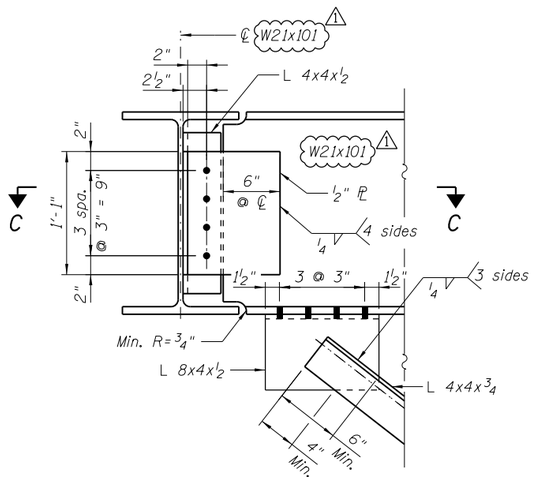
PLAN



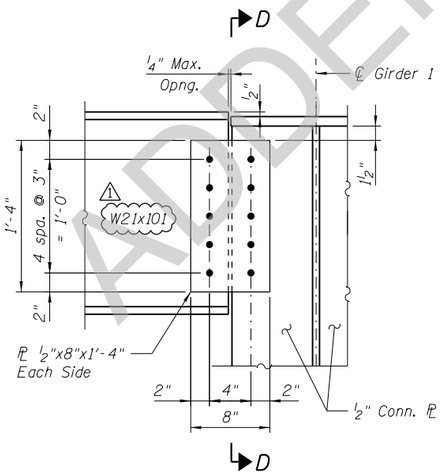
SECTION A-A



SECTION B-B



DETAIL A



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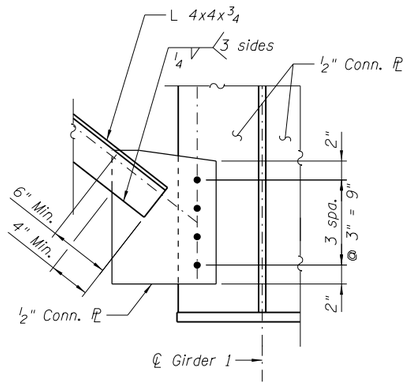

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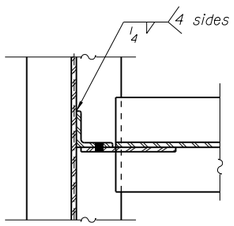
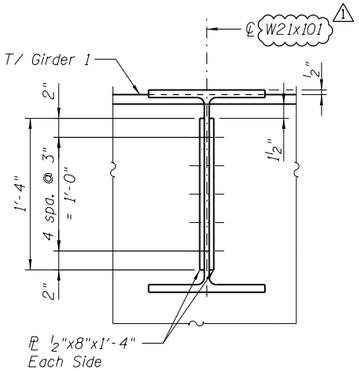


SECTION C-C



SECTION D-D

NOTES:

1. Detail $1\frac{1}{16}$" diameter holes for all $\frac{7}{8}$" diameter bolts.
2. Two hardened washers required for each set of oversized holes.

CMT	USER NAME = Prabhu Ananthan	DESIGNED - CPB	REVISED - ⚠1 9/25/18
License No. 184-000613		DRAWN - CPB	REVISED -
	PLOT SCALE = 4.0002 ' / in.	CHECKED - PA	REVISED -
	PLOT DATE = 9/24/2018	DATE - 8/31/2018	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

BALCONY FRAMING DETAILS
STRUCTURE NO. 045-3024

SCALE: | SHEET 32 OF 90 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2298	13-00215-20-BR	KANE	239	126
		CONTRACT NO. 63955		
		ILLINOIS FED. AID PROJECT		

BENCHMARK: Chiseled "□" S.E. corner of concrete slab. The Intersection of ILL-31 and Miller Rd. Go N. 0.9 MI ± to Mark. Elev. 806.34

EXISTING STRUCTURE: None

NOTES:

1. Wall Stations and Offsets are given to the Front Face (FF) of the wall and are measured from centerline of Longmeadow Pkwy.
2. Wall is built in conjunction with new bridge S.N. 045-3024.
3. Horizontal dimensions measured along front face of precast panels.
4. See roadway plans for embankment, removal of unsuitable soil, and rockfill quantities.

WATERWAY INFORMATION

Drainage Area = 1415 sq. mi. Low Grade Elev. 766.53 @ Sta. 2215+96.51

Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft. Exist.	Opening Sq. Ft. Prop.	Nat. H.W.E.	Head - Ft. Exist.	Head - Ft. Prop.	Headwater El. Exist.	Headwater El. Prop.
	10	5,775	-	3,217	727.31	0.00	0.00	0.00	727.31
Design	50	8,345	-	4,373	728.85	0.00	0.01	0.00	728.86
Base	100	10,095	-	5,044	729.73	0.00	0.01	0.00	729.74
Overtopping	-	-	-	-	-	-	-	-	-
Max. Calc.	500	12,525	-	5,892	730.81	0.00	0.00	0.00	730.81

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications
7th Edition with 2015 Interims

DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)

PRECAST UNITS

f'c = 4,500 psi (Precast Panels)
fy = 60,000 psi (Reinforcement)

SOIL BORINGS

	BORING	LOCATION
	S-092	2205+66.13, 30.03' Rt.
	S-093	2205+65.65, 30.00' Lt.
	S-094	2206+85.26, 29.96' Rt.
	S-095	2206+85.66, 30.06' Lt.
	BSB-01	2207+93.15, 44.06' Lt.
	BSB-02	2208+18.03, 20.27' Rt.
**	S-096	2208+76.40, 10.17 Rt.

** Not shown on plan view

WALL 1 BILL OF MATERIAL

	ITEM	UNIT	QUANTITY
	Stone Riprap, Class A4	Sq. Yd.	115
	Filter Fabric	Sq. Yd.	115
	Structure Excavation	Cu. Yd.	1,126
	Concrete Superstructure	Cu. Yd.	346.6
	Protective Coat	Sq. Yd.	682
	Reinforcement Bars, Epoxy Coated	Pound	52,220
	Pedestrian Railing	Foot	213
	Parapet Railing	Foot	216
	Mechanically Stabilized Earth Retaining Wall	Sq. Ft.	12,601
*	Anti-Graffiti Protection System	Sq. Ft.	8,667
*	Staining Concrete Structures	Sq. Ft.	8,667

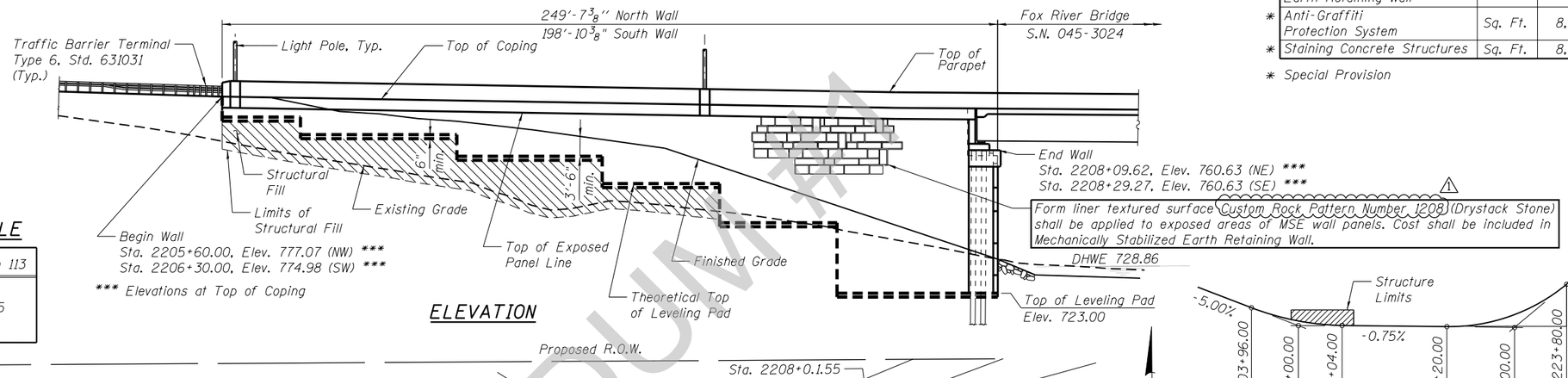
* Special Provision

LEGEND

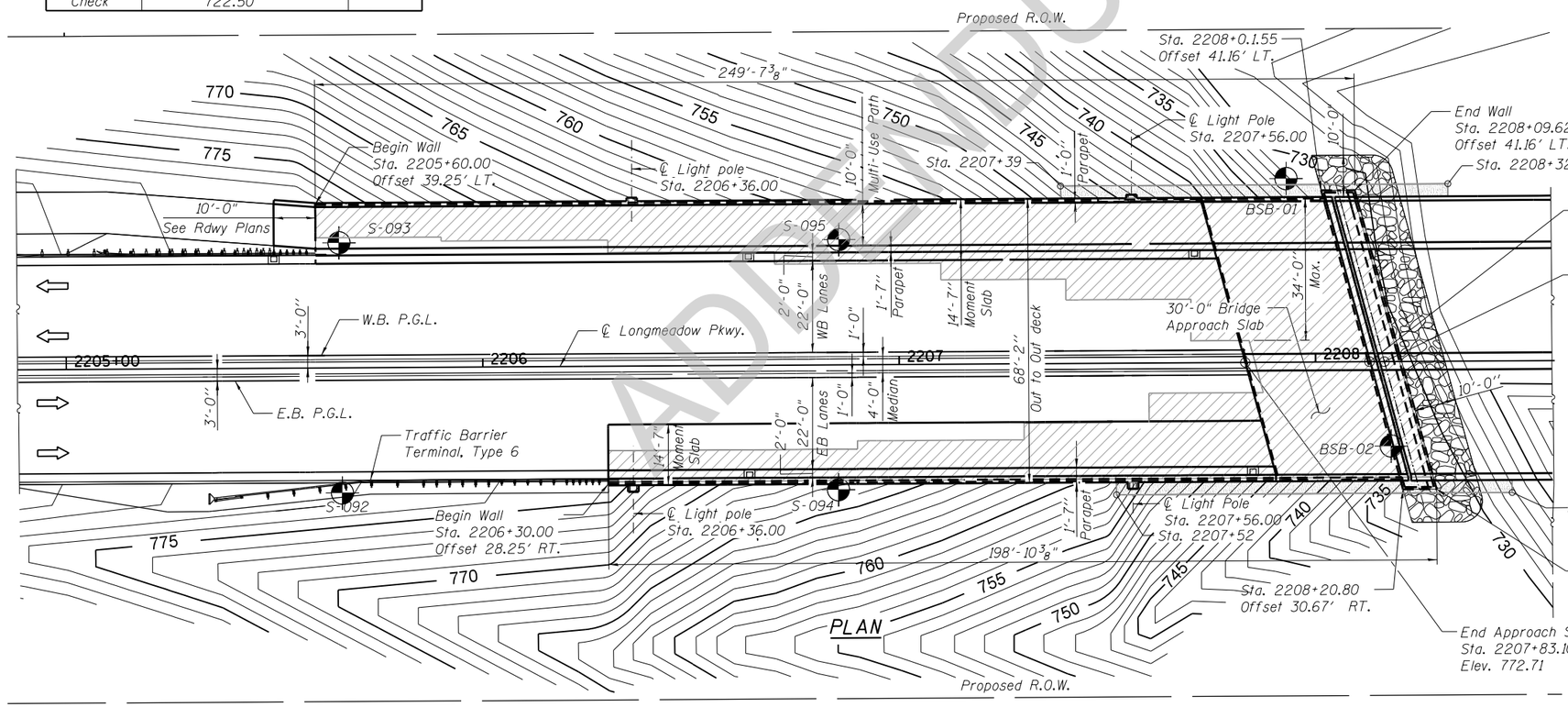
- Approximate Limits of Reinforced Soil Mass of MSE Retaining Wall
- Approximate Limits of Removal of Unsuitable Soil
- Approximate Limits of Stone Riprap, Class A4
- Approximate Limits of Structural Fill
- Proposed Drainage Structure

DESIGN SCOUR ELEVATION TABLE

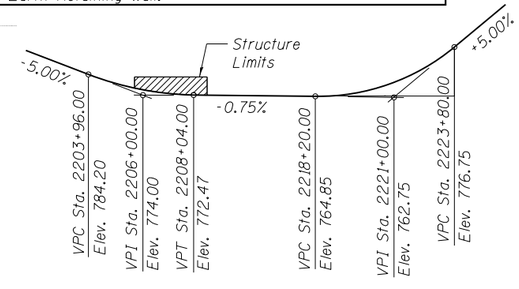
Event/Limit State	Design Scour Elevations (ft.) W. Abut.	Item 113
Q100	723.60	5
Q500	723.40	
Design	722.50	
Check	722.50	



ELEVATION

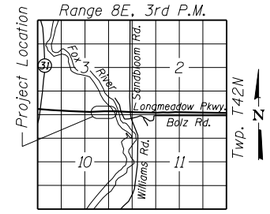


PLAN



PROFILE GRADE

(Along Longmeadow Parkway)
(3'-0" Offset from ℄ Longmeadow Parkway)



LOCATION SKETCH

GENERAL PLAN
LONGMEADOW PARKWAY OVER FOX RIVER
PUBLIC WATER
SEC 13-00215-20-BR
KANE COUNTY
WEST ABUTMENT MSE WALL 1
STATION 2205+60.00 TO 2208+28.86

LOCAL COORDINATE SYSTEM

CMT License No.: 184-000613 © Copyright CMT, Inc.

USER NAME = Prabhu Ananthan	DESIGNED - PA	REVISED - 1 9/25/18
	DRAWN - DWL	REVISED -
PLOT SCALE = 33.3348 ' / in.	CHECKED - PA	REVISED -
PLOT DATE = 9/24/2018	DATE - 8/31/2018	REVISED -

KANE COUNTY DIVISION OF TRANSPORTATION

WALL 1 – GENERAL PLAN & ELEVATION
STRUCTURE NO. 045-3024

SCALE: | SHEET 47 OF 90 SHEETS | STA. 2205+60.00 TO STA. 2221+10.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2298	13-00215-20-BR	KANE	239	141
		CONTRACT NO. 63955		
		ILLINOIS FED. AID PROJECT		

BENCHMARK: Chiseled "□" S.E. corner of concrete slab. The Intersection of ILL-31 and Miller Rd. Go N. 0.9 MI ± to Mark. Elev. 806.34

EXISTING STRUCTURE: None

NOTES:

1. Wall Stations and Offsets are given to the Front Face (FF) of the wall and are measured from centerline of Longmeadow Pkwy.
2. Wall is built in conjunction with new bridge S.N. 045-3024 and bridge S.N. 045-3077.
3. Horizontal dimensions measured along front face of precast panels.
4. See roadway plans for embankment, removal of unsuitable soil, and rockfill quantities.

SOIL BORINGS

BORING	LOCATION
S-104	2216+27.49, 33.12' Rt.
S-105	2216+25.34, 30.17' Lt.
BSB-03	2216+78.07, 1.00' Rt.
S-106	2217+44.06, 29.19' Rt.
S-107	2217+45.58, 29.94' Lt.

WATERWAY INFORMATION

Drainage Area = 1415 sq. mi. Low Grade Elev. 766.53 @ Sta. 2215+96.51

Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft. Exist.	Opening Sq. Ft. Prop.	Nat. H.W.E.	Head - Ft. Exist.	Head - Ft. Prop.	Headwater El. Exist.	Headwater El. Prop.
	10	5,775	-	3,217	727.31	0.00	0.00	0.00	727.31
Design	50	8,345	-	4,373	728.85	0.00	0.01	0.00	728.86
Base	100	10,095	-	5,044	729.73	0.00	0.01	0.00	729.74
Overtopping	-	-	-	-	-	-	-	-	-
Max. Calc.	500	12,525	-	5,892	730.81	0.00	0.00	0.00	730.81

WALL 2
BILL OF MATERIAL

	ITEM	UNIT	QUANTITY
	Stone Riprap, Class A4	Sq. Yd.	428
	Filter Fabric	Sq. Yd.	428
	Structure Excavation	Cu. Yd.	2,066
	Concrete Superstructure	Cu. Yd.	184.6
	Protective Coat	Sq. Yd.	348
	Reinforcement Bars, Epoxy Coated	Pound	27,880
	Parapet Railing	Foot	110
	Pedestrian Railing	Foot	107
	Mechanically Stabilized Earth Retaining Wall	Sq. Ft.	18,078
*	Anti-Graffiti Protection System	Sq. Ft.	15,538
*	Staining Concrete Structures	Sq. Ft.	15,538

* Special Provision

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications
7th Edition with 2015 Interims

DESIGN STRESSES

FIELD UNITS

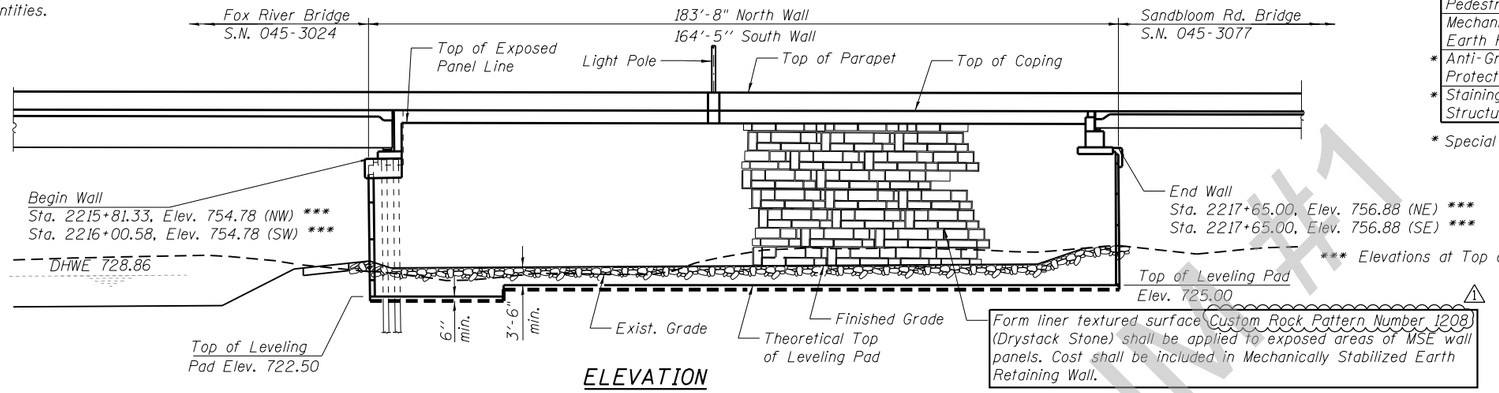
f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)

PRECAST UNITS

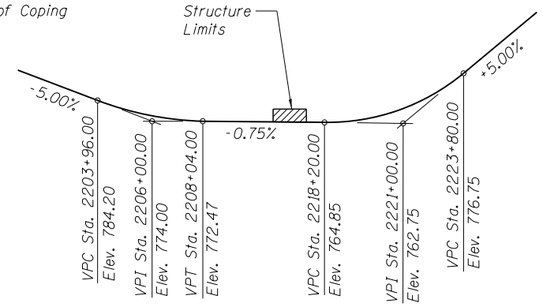
f'c = 4,500 psi (Precast Panels)
fy = 60,000 psi (Reinforcement)

DESIGN SCOUR ELEVATION TABLE

Event/Limit State	Design Scour Elevation (ft.) E. Abut.	Item 113
Q100	722.71	5
Q500	722.51	
Design	722.00	
Check	722.00	

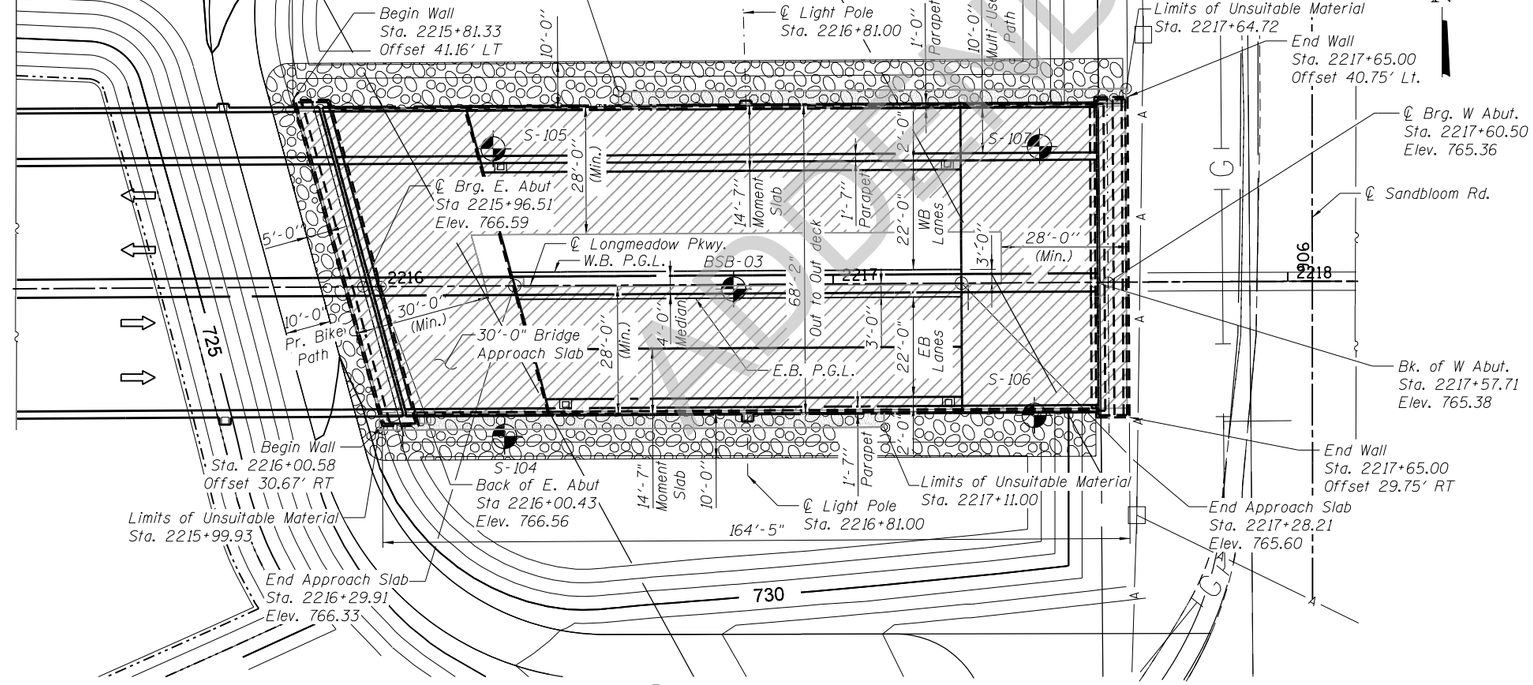


ELEVATION



PROFILE GRADE

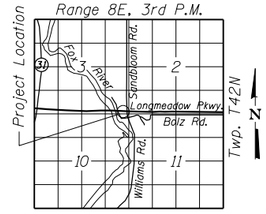
(Along Longmeadow Parkway)
(3'-0" Offset from ℄ Longmeadow Parkway)



PLAN

LEGEND

- Approximate Limits of Reinforced Soil Mass of MSE Retaining Wall
- Approximate Limits of Removal of Unsuitable Soil
- Approximate Limits of Stone Riprap, Class A4
- Soil Boring
- A — Existing Aerial Line to be removed within this contract
- G — Existing Gas Line to be removed within this contract
- Proposed Drainge Structure



LOCATION SKETCH

GENERAL PLAN
LONGMEADOW PARKWAY OVER FOX RIVER
PUBLIC WATER
SEC 13-00215-20-BR
KANE COUNTY
MSE WALL 2
STATION 2215+81.33 TO 2217+65.00

LOCAL COORDINATE SYSTEM

USER NAME = Prabhu Ananthan	DESIGNED - PA	REVISED - 1 9/25/18	
	DRAWN - DWL	REVISED -	
PLOT SCALE = 33.3349 ' / in.	CHECKED - PA	REVISED -	
PLOT DATE = 9/24/2018	DATE - 8/31/2018	REVISED -	

CMT License No. 184-000613

KANE COUNTY
DIVISION OF TRANSPORTATION

WALL 2 – GENERAL PLAN & ELEVATION
STRUCTURE NO. 045-3024

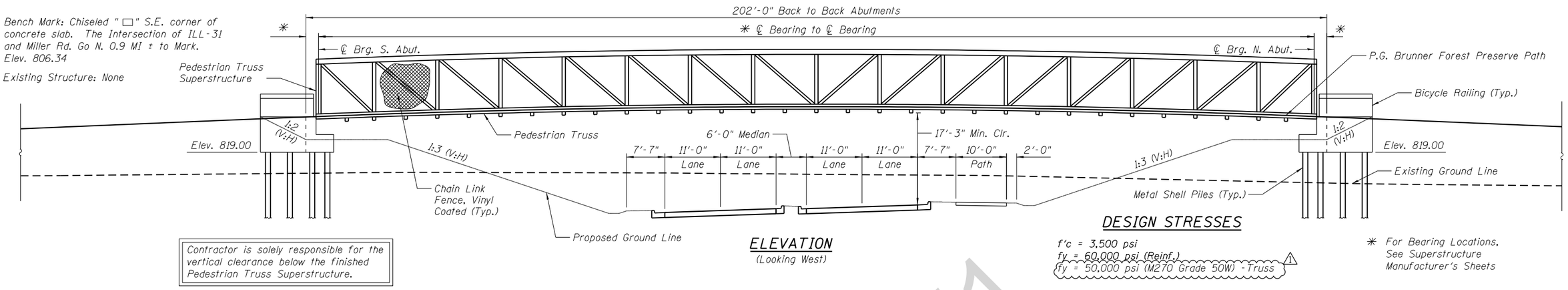
SCALE: | SHEET 64 OF 90 SHEETS | STA. 2205+60.00 TO STA. 2221+10.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2298	13-00215-20-BR	KANE	239	158
		CONTRACT NO. 63955		
	ILLINOIS FED. AID PROJECT			

FILE NAME = pw:\\centengr-pw.bentley.com:cmt-projects\Documents\Projects\Kane(D)\13296-02\Draw\Bridge\CADD_Sheets\045-W002 MSE Wall 2\SectionC-sht-sur-045-w002-MSE_WALL_001_cmt.dgn

Bench Mark: Chiseled "□" S.E. corner of concrete slab. The Intersection of ILL-31 and Miller Rd. Go N. 0.9 MI ± to Mark. Elev. 806.34

Existing Structure: None



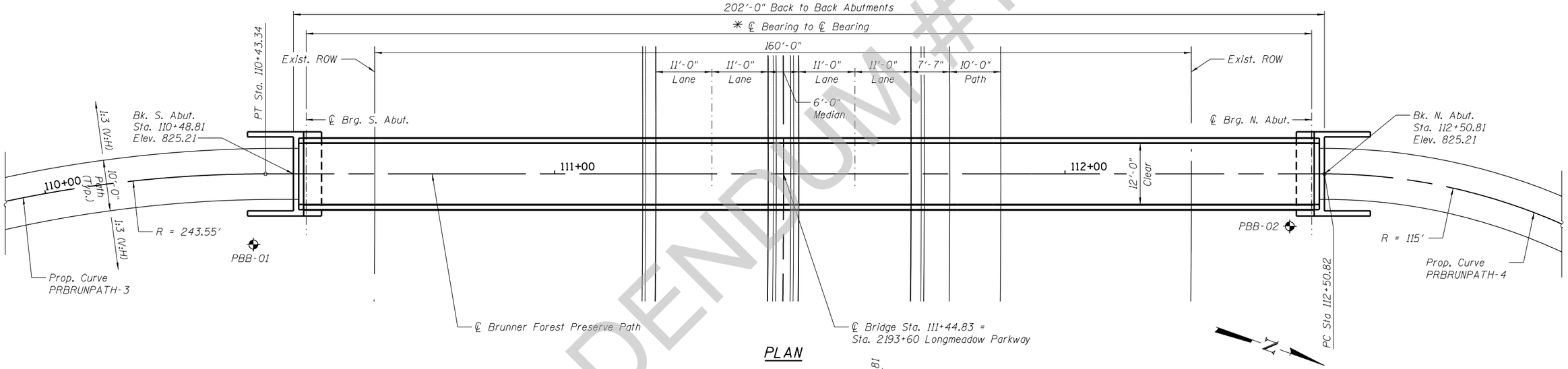
ELEVATION
(Looking West)

Contractor is solely responsible for the vertical clearance below the finished Pedestrian Truss Superstructure.

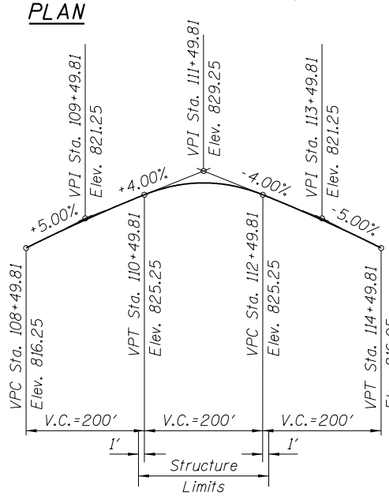
DESIGN STRESSES

f'c = 3,500 psi
fy = 60,000 psi (Reinf.)
fy = 50,000 psi (M270 Grade 50W) -Truss ⚠

✳ For Bearing Locations, See Superstructure Manufacturer's Sheets



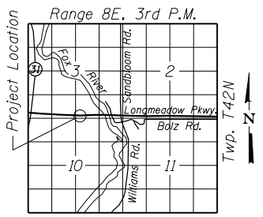
PLAN



PROFILE GRADE
(Along ℄ of Brunner Preserve Path)

SOIL BORINGS

BORING	LOCATION
PBB-01	110+40.82, 13.84' Rt.
PBB-02	112+44.02, 10.25' Rt.



LOCATION SKETCH

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition

AASHTO LRFD Guide Specifications for the Design of Pedestrian Bridges, 2nd Edition

SEISMIC DATA

Seismic Performance Zone (SPZ)=1
Design Spectral Acceleration at 1.0 sec. (S_{D1})=0.082
Design Spectral Acceleration at 0.2 sec. (S_{DS})=0.144
Soil Site Class = D

CLASSIFICATION

Pedestrian/Bicycle Bridge

LIVE LOADING

90 psf Pedestrian Live Load
20.000 lb. Vehicle Load (H10 Truck)

PROP. CURVE PRBRUNPATH-3
PI STA. = 109+56.48
Δ = 43° 00' 21" (RT)
D = 23° 31' 30"
R = 243.55'
T = 95.95'
L = 182.81'
E = 18.22'
P.C. STA. = 108+60.53
P.T. STA. = 110+43.34
P.T. NORTHING = 1,993,314.1834
P.T. EASTING = 997,082.4299

PROP. CURVE PRBRUNPATH-4
PI STA. = 113+13.49
Δ = 57° 10' 38" (RT)
D = 49° 49' 21"
R = 115.00'
T = 62.67'
L = 114.76'
E = 15.97'
P.C. STA. = 112+50.82
P.T. STA. = 113+65.58
P.C. NORTHING = 1,993,509.6366
P.C. EASTING = 997,012.8062

STATE OF ILLINOIS
JEFFREY A. RUHDE
081-005613
LICENSED STRUCTURAL ENGINEER

Jeffrey A Ruhde 8/31/18

ADDENDUM #1

GENERAL PLAN
BRUNNER FOREST PRESERVE PATH OVER LONGMEADOW PARKWAY
SEC 13-00215-20-BR
KANE COUNTY
STATION 111+44.83
SN. 045-3532

FILE NAME = L:\KANECO\13296-02_LongmeadowPkwy\Draw\CADD_Sheets\SectionC-ahs S-01-Ped Bridge_General Plan & Elevation.dgn

BURNS McDONNELL

USER NAME = Mike Moes	DESIGNED - XXX	REVISED - ⚠1 9/25/18	
	DRAWN - XXX	REVISED -	
PLOT SCALE = 2.0000 ' / in.	CHECKED - XXX	REVISED -	
PLOT DATE = 9/24/2018	DATE - 8/31/2018	REVISED -	

KANE COUNTY
DIVISION OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 045-3532

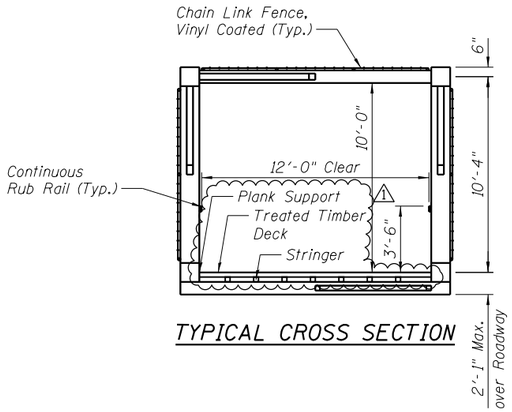
SCALE: 1" = 20' | SHEET 1 OF 7 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2298	13-00215-20-BR	KANE	239	185
		CONTRACT NO. 63955		
	ILLINOIS FED. AID PROJECT			

GENERAL NOTES

1. Fasteners shall be ASTM A325 Type 1, mechanically galvanized bolts (in painted areas and ASTM A325 Type 3 in unpainted areas). Bolt size shall be determined by the Contractor.
2. No field welding is permitted except as specified in the contract documents.
3. Reinforcement bars designated (E) shall be epoxy coated.
4. Concrete Sealer shall be applied according to the limits shown.

Contractor is solely responsible for the vertical clearance below the finished Pedestrian Truss Superstructure.



TYPICAL CROSS SECTION

BRUNNER FOREST
PRESERVE PATH
BUILT 2018 BY
KANE COUNTY
SEC. 13-00215-20-BR
STATION 111+49.81
STR. NO. 045-3532
LOADING H-10

NAME PLATE
See Std. 515001

TRUSS MANUFACTURER

The substructure is designed per AASHTO LRFD and based on the assumed truss dead loads (including deck) shown under Brdige Reactions Table.

The truss manufacturer shall camber the truss to achieve the vertical curve shown in the Profile Grade diagram on General Plan & Elevation sheet after the full dead load deflection of truss and deck weight.

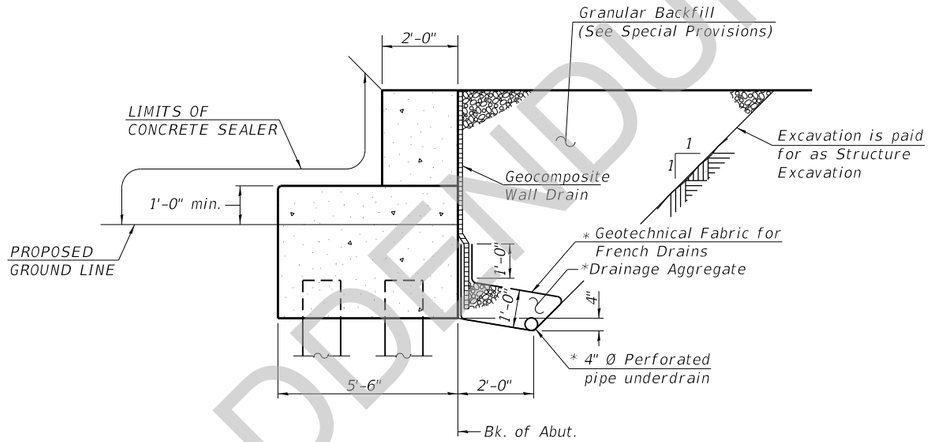
Bridge bearing seat elevations are subject to revision based on the approved pedestrian truss superstructure shop drawings. Contractor shall verify all dimensions and elevations with final shop drawings.

In no case shall a bearing seat adjustment result in a final vertical clearance of less than 17'-3".

The truss manufacturer shall design and furnish all truss bearing anchor bolts.

Bridge decking shall be nominal 3" thick select structural Timber in accordance with Section 1007.03 of IDOT Standard Specifications. Fbo 1.40 ksi minimum. Maximum stringer spacing 1'-8".

The truss manufacturer shall design connection of timber deck to stringers and submit sealed calculations with shop drawings. 1



SECTION THRU PILE SUPPORTED STUB ABUTMENT

(Horiz. dim. @ Rt. L's)

*Included in the cost of Pipe Underdrains for Structures. (See Special Provisions)

Note:
All drainage system components shall extend parallel to the abutment back wall until they intersect the wingwalls or 2'-0" from the end of the wingwalls when the wings are parallel to the abutment. The pipe shall extend under the wingwall, if necessary, until intersecting the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structure Excavation	Cu. Yd.	93.4
Concrete Structures	Cu. Yd.	44.8
Reinforcement Bars, Epoxy Coated	Pound	4940
Furnishing Metal Pile Shells 12" x 1/4"	Foot	741
Driving Piles	Foot	741
Test Pile Metal Shells	Each	2
Concrete Sealer	Sq. Ft.	374
Geocomposite Wall Drain	Sq. Yd.	45.8
Pedestrian Truss Superstructure	Sq. Ft.	2424
Granular Backfill for Structures	Cu. Yd.	32.4
Pipe Underdrains for Structures, 4"	Foot	64
Name Plate	Each	1
Bicycle Railing	Foot	40

BRIDGE REACTIONS TABLE

LOAD (+Downward load, -Upward load)	P (LBS)	H (LBS)	L (LBS)
Dead Load	50,000	-	-
Uniform Live Load	53,460	-	-
Vehicle Load	15,500	-	-
Wind Uplift 20 PSF	-12,400	-	-
Wind	±19,400	39,810	-
Thermal	-	-	3,500

"P" - Vertical load each base plate (4 per bridge)
"H" - Horizontal load each footing (2 per bridge)
"L" - Longitudinal load each base plate (4 per bridge)

FILE NAME = L:\KANECO\13296-02_Longmeadow_Pkwy\Draw\CADD_Sheets\Section_Sht S-02-Ped Bridge_General Notes & Quantities.dgn

BURNS McDONNELL

USER NAME = Mike Moes	DESIGNED - XXX	REVISED - 1 9/25/18
	DRAWN - XXX	REVISED -
PLOT SCALE = 2.0000 ' / in.	CHECKED - XXX	REVISED -
PLOT DATE = 9/24/2018	DATE - 8/31/2018	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

GENERAL NOTES AND QUANTITIES
STRUCTURE NO. 045-3532

SCALE: 1" = 20' | SHEET 2 OF 7 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2298	13-00215-20-BR	KANE	239	186
		CONTRACT NO. 63955		
	ILLINOIS FED. AID PROJECT			